



Development Services
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City of Issaquah
Development Services Department
Administrative Site Development Permit

NOTICE OF DECISION

TO: Scott Clark
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SUBJECT: Administrative Site Development Permit for Block 19 of the Issaquah Highlands

FILE NO.: ASDP17-00004

Decision Date: October 13, 2017

Request: Application for an Administrative Site Development Permit for construction of a five-story, 135-unit apartment building with a two-level garage tucked under the building. Site improvements would include a covered structure with community amenities, entry courtyard, recreational amenity courtyard, generator enclosure, and landscaping on a total site area of 94,511 sq. ft. (2.17 acres).

Location: Northeast corner of 10th Avenue NE and NE Falls Drive, on Block 19 in Development Area 4 (Parcel No.: 3630360040)

Decision: On October 13, 2017 the Development Services Department approved the Administrative Site Development Permit application. Approval of this application is based on the April 21, 2017 submittal and the revised site plan received via email on September 6, 2017, elevations dated July 11, 2017, and preliminary landscape plans dated August 1, 2017, and is subject to the following conditions:

NOTES

1. If any of the Conditions or portion thereof is declared invalid or unenforceable, the application must be remanded to the Responsible Official for reconsideration and evaluation for consistency and appropriateness of the remaining Conditions.
2. Appeals may be filed by the applicant, the City Responsible Official, property owners within three hundred feet of the proposed action, or other persons claiming to be directly harmed by the proposed action within 14 calendar days after the Responsible Official issues the decision and mails it to the applicant. Appeals shall meet the Criteria identified in Section 8 of Appendix L (Processing) of the Development Agreement and follow the process identified in that Section.
3. Any major change (as determined by the Responsible Official) to the approved site plan or accompanying submittal information may require a revision to this ASDP or a new ASDP review and decision. Less substantial changes may be approved administratively by the Responsible Official.
4. Site Work, Landscape, Building and/or Sign Permits will not be approved unless all applicable Conditions, including Construction Conditions (Attachment 3), of this Notice of Decision are satisfied to the satisfaction of the Responsible Official.
5. This action does not indicate nor imply that any development activities may occur without the required permits being issued.
6. This ASDP approval expires three years from the Decision Date or as otherwise provided by IMC 18.04.220.

Site

1. The design of some portion of the building and/or site shall facilitate active use for all ages. In the Site Work, Landscape, and/or Building Permit, incorporate elements into an area of the building, such as the common room or terrace, to encourage children's play. These elements shall be both indoors and outdoors, and do not have to be overtly for play but should support, allow, and even encourage play by children.
2. The covered structure at the northeast corner of the site is intended to be a community amenity, and will include a dog station, seating, bicycle parking, and a bicycle repair station. This structure and its amenities shall be maintained by the property owner, and shall be accessible to residents, their guests, and the general public. An easement shall be granted to the City to ensure public access to the covered structure.
3. All exterior staircases and walkways shall be at least 5 feet wide, clear of obstructions (e.g., handrails, mature landscape, car overhangs, light poles, tables and chairs, etc.), except walkways to individual front doors or individual patios serving only one residence, which may be 3 feet wide. All pedestrian pathways must be paved in concrete, pavers, or other pedestrian-friendly materials and shall not be paved in asphalt or gravel, except the pathway in or immediately adjacent to the BSBL which shall be gravel or concrete. There shall be at least two steps in each stair and they shall be generally level. For planter beds adjacent to walkways, plants whose mature size will not impact the walkway width shall be selected.
4. A minimum 5-foot wide trail adjacent to the wetland shall be provided for a pedestrian connection between the WSDOT pond trail and 10th Avenue NE, as follows:
 - the on-site wetland trail may be gravel; however, use of gravel surfacing is not allowed within 20 feet of the WSDOT pond trail and public right-of-way, where paved trail connections shall be required;
 - the paved trail connection to the WSDOT pond trail at the northeast corner of the site shall be limited to 20 feet in length if paved in asphalt; if the Issaquah Highlands Community Association (IHCA) will be maintaining the trail adjacent to the pond, they shall have input on the trail surface;

- the paved trail connection from the public sidewalk along 10th Avenue NE at the northwest corner of the site shall be concrete;
- the on-site wetland trail and trail connections may be widened to 12 feet to accommodate bicycles; and,
- an easement shall be granted to the City to ensure public access of the on-site wetland trail and trail connections.

Urban and Building Design

5. At least one primary entry for a ground floor residential unit shall be provided on the south elevation along NE Falls Drive, in order to provide a residential presence along this frontage.
6. Blank walls shall be avoided, especially at the pedestrian's level; if necessary, articulation or other features shall be provided. This standard will be met through the use of materials, articulation, details, artwork, etc. that adds texture and visual interest to the wall to create an interesting and varied environment. Long, unbroken use of a single material will not meet this standard. Landscaping may also be applied as a secondary feature of the building. Areas with blank walls where this standard would apply include, but are not limited to, the following:
 - exterior edges of the parking garage on the north and south elevations, and any ventilation grilles shall incorporate a visually interesting design (i.e., not standard vertical/horizontal slats, louvers, or grids);
 - wall at the trash room on the south elevation;
 - wall section to the north of the pool on the east elevation; and,
 - north-facing wall in the entry courtyard.
7. Large prominent doors along NE Falls Drive for the exterior trash room and at both garage entrances shall incorporate visually interesting designs (i.e., not standard metal roll-up doors) that are integrated with the design of the building.
8. Most units shall have balconies or other semi-private outdoor space.
9. The covered structure at the northwest corner of the site shall be designed to complement the main residential building. Additionally, this structure shall provide weather protection, provide a high level of transparency from the street, and provide access to natural light. Solid or blank walls are not allowed.
10. Prior to submitting construction permits except grading, determine where United States Postal Service will require Block 19's mailbox kiosk and incorporate it into the site and building design.
11. The generator shall be fully screened from view, including views from above. The screening material shall not be wood and shall not be visually permeable.

12. All HVAC and mechanical equipment, including rooftop-mounted equipment, shall be fully screened from views from above and below.

Landscape

13. This is a preliminary landscape review. More detailed review will occur with the Landscape and/or Building Permit, and will occur jointly with the IHCA Architectural Review Committee (ARC). The following are preliminary landscape comments:
 - landscape for the site must comply with the Traditional Townscape standards (i.e., formal structured and urban in character with respect to configuration, species, durability, etc., and provide year-round greenery);
 - a minimum landscape buffer of 3 feet in height and width planted with hedges is required between pedestrian and parking areas;
 - the landscape on the eastern and northern edges of the property may deviate from the strict Traditional Townscape character as it transitions to NF 34's wetland buffer and approved pond landscape;
 - generally, evergreen trees should only be used to transition to the wetland and pond, as a complement to their landscape character; and,
 - landscape drawings shall show plants at 85% maturity.
14. New street trees shall be planted near the driveways along Falls Drive, consistent with the existing street tree species. To the extent possible, the spacing for the street trees should maintain the existing street tree spacing provided that tree locations will not adversely impact driver visibility and pedestrian safety.
15. Prior to the installation of any plant material, documentation shall be provided to the City that the associated irrigation plans have been reviewed by the IHCA (or equivalent as determined by the Responsible Official) and are consistent with the water budget indicated in the plan set; and, are consistent with the Issaquah Highlands Water Conservation Standards. Compliance with the Issaquah Highlands Water Conservation Standards shall be evaluated as part of the Landscape Permit.
16. The landscape Performance Bond shall not be released without a letter of acknowledgement from the IHCA stating the landscaping will be maintained by the IHCA. If the landscape will not be maintained by the IHCA, a Maintenance Bond is necessary prior to the release of the Performance Bond and Certificate of Occupancy. The Performance Bond shall be paid prior to issuance of the Landscape Permit.
17. Landscaping for the utility vault area and generator enclosure at the northwest corner of the site, as well as the utility area to the southeast corner of the site, shall be designed in such a way as to obscure and distract from their presence.
18. Any trees located within 8 feet of a public sidewalk, curb, sidewalk, or similar publicly owned and maintained paving must have at least 10 lineal

feet of root barrier, or as recommended by root barrier manufacturer, placed adjacent to the pavement.

Parking

19. Bicycle parking is provided in the covered structure at the northwest corner of the site and in a bicycle storage room on level P1 in the main building. Verification that the number of bicycle parking spaces provided will comply with the code required minimum based upon the total number of vehicular parking stalls will occur with review of the Site Work and Building Permits.
20. Two loading spaces, consistent with Appendix O (Parking), shall be provided. The dimensions of the on-street loading space on NE Falls Drive shall be determined during review of the Site Work Permit, and shall be based upon the minimum length needed to accommodate up to two waste containers and for waste collection truck access.
21. The number, type, and location of ADA compliant parking spaces is not approved by this permit and shall be reviewed through the Site Work and/or Building Permit. The applicant should meet with the Building Official prior to the submittal of any construction permits to confirm the number and distribution of ADA parking stalls.
22. In the parking garage, drives and drive aisles, where cars will not be backing out, will be only 20 feet wide; where all standard/ADA or a combination of standard/ADA and compact stalls are located, drive aisles will be 24 feet wide; where only compact stalls are located on a drive aisle, it may be reduced to 22 feet, though for design simplicity the drive aisle may be 24 feet wide, but no wider.
23. Provide a total of 233 vehicular parking spaces or parking as required by final unit mix. Up to 9 on-street parking spaces along the site's frontage may be counted towards the parking requirement, and is based on 20 linear feet of curb where on-street parking is allowed adjacent to the site. Parking considerations that either reduce parking demand or increase parking supply must be reviewed and approved by DSD.
24. Wheel stops shall not be installed at the parking stalls in the entry courtyard. Other mechanisms such as bollards or curbs shall be used to help delineate the extent of these parking stalls.
25. Bicycle racks shall be positioned to not block sidewalk, walkways, entrances, etc., when full of bicycles. Final bicycle rack locations must be shown on Building and/or Site Work Permits. A portion of the bicycle racks should be located in the garage for tenant use and a portion of the bicycle racks should be located for use by residential guests or the public.
26. Adopted standard stall dimensions shall be the maximum; adopted compact stall dimensions shall be the minimum. Stalls smaller than standard stall dimensions, in one or both directions, shall be considered compact stalls.

Roads and Drives

27. All vehicular entries to the site must be designed as driveway entrances using standard curb ramps. All grade transition for the driveway entry shall occur in the planter strip area. Driveways are limited to 20 feet with a 2-foot wing on either side. Vehicular entries shall be designed and sited to eliminate, or if necessary, minimize the impact to existing street trees. Landscaped bulb-outs on the outer edges of the driveways on NE Falls Drive and between the two driveways are required for the purposes of improving safety/visibility and to allow additional landscaping, including the planting of street trees.
28. Garage doors/gates restricting access to the parking garage must be sufficiently set back to allow one entering vehicle to wait in the driveway in front of the garage door without obstructing the street and sidewalk.
29. Garage ramps near the driveway shall be at a slope that would give drivers adequate sight lines of oncoming pedestrians and vehicular traffic to ensure safety.
30. "No Parking" or "No Parking, Fire Lane" signs, as appropriate, shall be installed during construction.
31. All curbs at Issaquah Highlands must be vertical, unless otherwise approved by the DSD such as for fire access or some other unique circumstance. No extruded curbs are allowed.
32. All curb ramps must direct the user into the crosswalk (not the intersection or travel lanes) and generally point toward the curb ramp on the opposing side. The applicant shall use truncated domes per WSDOT and ADA guidelines.
33. Parking in the entry courtyard along 10th Avenue NE will be load/unload only, with the exception of the two designated parking stalls.

Utilities

34. Storm water design shall comply with the 2009 King County Drainage Manual and the City of Issaquah 2011 Addendum.
35. Unused utilities stubs shall be removed and or abandoned per City Standards.
36. All dry and wet utility vaults, meters, equipment, and appurtenances are assumed to be shown on the ASDP submittal. Anything not shown on the ASDP submittal is assumed to be located within a structure. Any revisions or additions to what the ASDP has shown and approved outside of the structure requires a modification to the ASDP, except fire hydrants.
37. Design and placement of the above ground facilities, such as building entries, walkways, significant plant materials, etc... shall take priority over

the convenient location of utilities, unless this would significantly compromise the function of the utilities. On all subsequent permits, utilities and their necessary easements shall be shown.

38. In no way shall the dry utility location impact or drive building placement on the site. Dry utilities shall be shown on Site Work, Landscape, and Building Permits to ensure these utilities are not driving building placement.

39. Any grade changes (e.g., slopes, walls, etc.) over 2.5 feet in height which have pedestrians within 3 feet require 42-inch fall protection.

40. Exterior lighting shall be reviewed with the Site Work, Landscape, and/or Building Permit. A lighting plan shall be submitted which maintains lighting at the minimum necessary for safety, and balances the goal of minimizing night glow and off-site lamp visibility with pedestrian scale lighting. Uplighting is not permitted. In particular, with regards to adjacent areas especially residences, the garage shall be designed to:

- ensure no direct spilling of light;
- minimize reflective light and exterior glare spilling from the garage; and,
- eliminate or significantly reduce visibility of pin point light sources. This may include limiting openings, screening openings with architectural and/or landscape elements, fixture selection (e.g. cut off, lenses), fixture location, turning off fixtures late at night/early in the morning, etc.

The lighting plan shall comprehensively address building, street, courtyards, parking lot, and landscape lighting so that lighting impacts are not compounded in portions of the site by overlapping illumination patterns. To facilitate review of the lighting, a point-by-point photometric calculation showing illumination levels on the pavement shall be submitted with the Site Work, Landscape and/or Building Permit for construction of lighting. The illumination calculation shall include all fixtures that contribute light to the site (poles, bollards, building mounted lighting). For roadways, include the next pole outside of project limits. If steps or significant changes in grade occur, this should be accurately modeled in the photometric calculation. Additionally, a fixture schedule, manufacturer's cut sheets for all fixture types, and fixture mounting details shall be provided. Low wattage decorative fixtures such as sconces or porch lights can be excluded from the calculation.

41. Oil/water separators that serve the parking garages must either be located within the garages or within paved rights-of-way so that they do not impact or reduce the amount of landscaping in planting areas or impinge on pedestrian areas.

42. When the ROW or tract property line is located at the back of a sidewalk, and it is determined to be necessary by the Responsible Official, a two-foot maintenance and repair easement shall be granted prior to Certificate of Occupancy for either building. For instance, if a building or wall is located at the back of sidewalk, the easement is not necessary.

43. The proposed generator must use non-diesel fuel. Noise attenuation may be required to comply with the City's noise standards.

Critical Areas

44. Prior to issuance of construction permits for areas including or adjacent to critical areas, permanent survey stakes shall be installed in the field that delineate the boundaries of all critical areas.
45. During any construction activities within 100 feet of a critical area, the applicant shall hire an independent qualified professional acceptable to the Responsible Official, to be on-site to ensure permitted activities do not exceed the limits indicated on these approved plans. Following completion of the approved activities in these areas, a licensed surveyor shall submit an affidavit to the Responsible Official attesting that the activity was maintained within approved limits. This affidavit shall be submitted to the Development Services Department prior to the certificate of occupancy of any Building Permits for the area in question.
46. Lights shall be directed away from wetlands. Light spillover within the wetland buffer is limited to 0.3 footcandles.
47. The use of hazardous or toxic substances and pesticides or certain fertilizers is prohibited in the 15-foot BSBL from stream and wetland buffers; organic, slow-release fertilizers are permitted.
48. Where critical areas are adjacent to the site, permanent signs identifying the type and value of the critical area shall be installed prior to occupancy of any adjacent buildings. Signs shall be placed one per 50 feet or as directed by the Responsible Official.
49. Retaining walls within the BSBL may not be taller than 4 feet in height and must be located and constructed so as to have no impacts on the buffer or wetland.
50. It does not appear that the site contains any steep slopes; however, no regulated steep slopes may remain or be created following construction.
51. Prior to the approval of construction permits, the applicant, successors or assignees shall gain approval from DSD for measures to include:
- methods for discouraging intrusions into the wetland following construction that may include fencing, such as a split rail fence; galvanized metal or copper are not allowed;
 - methods for ensuring construction impacts are minimized; and,
 - methods for clearly demarking the boundary between the critical area and private property.

Miscellaneous

52. Construction conditions are included in Attachment 3.

53. Signs will require a separate Sign Permit.

54. The applicant shall consider applying techniques identified in the current Issaquah Highlands green building program, or even attaining green building LEED certification.

55. The applicant shall work with the City and the City's waste removal company to ensure that the project can be reasonably served. Prior to submittal of Building or Site Work Permit, the applicant shall review the design with the City and City's waste removal company. Waste collection is preliminarily approved based on the assumptions given in "Reasons for Decision" and:

- waste collection facilities on each floor shall be designed to accommodate three waste streams (recycling, garbage, and food waste);
- an on- or off-site manager will ensure that containers are accessible for pick up on collection day and are returned to the trash rooms inside the building after pick up;
- no containers are allowed to be on the street overnight;
- if a cart and/or equipment is required to roll out the containers for pick up, the cart and/or equipment must be on-site with provisions for its storage outside of a required parking stall prior to Temporary Certificate of Occupancy;
- grades in the garage shall be designed to accommodate the path of travel for waste containers;
- the pick-up area will be designed to facilitate waste removal employees accessing the containers; and,
- pedestrian and vehicular safety will not be adversely affected when containers are placed on the street for pick up.

56. A Construction Impact Mitigation Plan (CIMP) shall be prepared that addresses the following:

- description of different stages of construction and associated activities and primary equipment(s) to be used at each stage;
- noise control measures for construction activities;
- air quality control measures, including off-site migration of dust and dirt from construction activities;
- laydown area, including construction worker parking, material and equipment storage;
- construction fencing plan;
- on-site circulation plan for construction vehicles;
- truck route plan;
- traffic control plan, including signage, flaggers, and plan to address any right-of-way closures; and,
- public outreach and site notification, including contact information for site supervisor.

The CIMP shall be approved prior to issuance of the first construction permit. Changes to construction activity may necessitate updating the CIMP.

57. Construction outreach shall include the following:

- on-site signage in conformance with the City's sign template, to be erected prior to start of any construction; and,
- monthly update sheet in conformance with the City's template, to be placed in a brochure box with the on-site sign and sent to members of the public who have signed up for updates throughout the duration of construction.

Reasons for Decision:

1. The purpose of the ASDP process is to ensure that proposed projects are:

- consistent with Issaquah Highlands Development Agreement and City policies and regulations;
- designed to harmonize with the neighborhood types;
- designed with consideration of the site attributes (topography, wetlands, waterways, and views);
- designed for the efficient and effective layout of the infrastructure; and,
- designed to achieve, protect, and enhance the character of Issaquah Highlands.

Finally, the planning level review of the ASDP allows an applicant to obtain a land use decision prior to preparing detailed construction plans. As such, the plans are intentionally conceptual in nature and will likely receive further refinement and development during development of construction plans. Additional permits will be required for signs, buildings, and on-site improvements and utilities.

2. The proposal is for the construction of a five-story, 135-unit apartment building located at Block 19 which contains 2.17 acres. The allowed density is 5-80 du/ac, and the proposed density is 62 du/ac. The use is also within the allowed range of residential, retail, recreation, and commercial uses that would be permitted on the site.
3. The proposal has been reviewed for its consistency with the Issaquah Highlands development standards. With incorporation of the conditions, the application is generally consistent with these standards, including goals (Appendix A), uses (Appendix B), stormwater (Appendix D), critical areas (Appendix E), roads (Appendix H), State Environmental Protection Act (Appendix I), heights and setbacks (Appendix N), parking (Appendix O), landscaping (Appendix P), urban design guidelines (Appendix S), and trails (Appendix T).
4. The proposal has also been reviewed for its consistency with previous land use permits. This block was included as part of the original land identified for the Grand Ridge/Issaquah Highlands Development in 1996. At the time that the Development Agreement was approved, Development Area 4 was divided into parcels generally 20 acres in size. In January 2002, a Site Development Permit for Microsoft's Issaquah Highlands' campus was approved (SDP01-001IH). The campus encompassed 150 acres including the land in this block. In 2004, Microsoft chose to not exercise its option on this property, thereby releasing this property for other uses. PP05-003IH, known as the East 42 plat, was approved in December 2005, and along with the final plat, FP06-002IH, created this block. A Site Development Permit for most of the East 42 plat area, known as High Streets, was approved in 2007 (SDP07-001IH). Finally, the design for the WSDOT pond to the east was approved in 2005 (ASDP05-008IH and PUB05-030IH). A previous Administrative Site Development Permit (ASDP08-005IH)

approved in 2008 for a 135-unit residential development expired on July 28, 2017. Either as shown in the proposal, discussed below, or addressed through conditions, the proposed development is consistent with relevant previous land use actions.

5. No phasing of improvements was indicated with the proposal. It is assumed the structures and all site improvements will be constructed in a single phase.
6. Setbacks are established in Appendix N (Land Use Dimension Standards), and are based on Development Area and land use. No minimum setbacks are required as there are no single family detached homes adjacent to this property.
7. The following table summarizes the required parking for the Block 19 proposal per the Development Agreement, Appendix O (Parking):

Use	Required Parking Spaces
10 studio units x 1 parking space/unit	10
36 1-bedroom units x 1.25 parking spaces/unit	45
89 2- and 3-bedroom units x 2 parking spaces/unit	178
<i>Total (135 units)</i>	<i>233</i>
<i>Total Required Parking</i>	<i>233 parking spaces</i>

Location	Provided Parking Spaces
On Site:	
garage level P1	100
garage level P2	106
entry courtyard	2
<i>Total On-Site parking</i>	<i>208</i>
Parking Credits:	
10th Ave. NE	2
NE Falls Drive	7
<i>Total On-street parking</i>	<i>9</i>
<i>Total Provided Parking</i>	<i>217 parking spaces</i>

While the proposal is 16 parking spaces short of the 233 spaces required, either a reduction in the parking demand or additional parking supply may be considered through the following methods:

- a change in the unit mix to incorporate more smaller units that could reduce the development's parking demand;
- reconfiguration of the parking garage layout to accommodate additional parking spaces; and,
- reduction in the build-out of on-site parking spaces through an Administrative Modification of Parking Standards, including, but not limited to, documentation of appropriate parking demand through a parking demand analysis; however, its granting is subject to review and is not guaranteed.

Compliance with parking requirements will be reviewed with Site Work and/or Building Permits.

Additional parking requirements:

On-street parking: up to 25% of required parking may be a parking credit on adjacent streets. The 9 on-street parking spaces, or 4%, that can be credited are significantly less than the allowed credit of up to 25%.

Loading: Two loading spaces are required to serve the development. One on-site loading stall is proposed in level P2, and would primarily serve residents' moving trucks. One on-street loading stall is proposed along NE Falls Drive, and would primarily serve waste collection vehicles and larger trucks that cannot be accommodated by the level P2 loading space. The dimensions of the on-street loading space shall be determined through Site Work Permit review based upon the minimum length needed to accommodate up to two waste containers and for waste collection truck access. The design of the on-street loading space shall facilitate container pick up without adversely affecting pedestrian and vehicular safety. Additionally, the entry courtyard along 10th Avenue NE is intended to accommodate short-term loading/parking for delivery trucks, mail carrier, ambulances, and passenger vehicles.

Bicycles: The proposed plans show bicycle parking being provided in the covered structure at the northwest corner of the site as well as a bicycle storage room on level P1, but does not specify the number of bicycle parking spaces provided at these locations. For every 12 required vehicular parking spaces, there must be one bicycle parking space. Based on the requirement for 233 vehicular parking spaces, a minimum of 20 bicycle parking spaces would be required. Bicycle parking spaces should serve the residents, residential guests, and on-site employees. Additionally, bicycle parking in the covered structure should be available for use by the general public. Placement of the bicycle racks shall not impinge on adjacent uses such as walkways, entrances, etc., when the racks are fully used. The proposed bicycle parking locations would encourage their use, and the bicycle storage room would allow for a secured, weather-protected area for long-term bicycle parking.

Compact: 101 stalls or 49% of the new stalls are shown on the plans as compact stalls, which is less than the 50-60% allowed.

ADA: DSD will determine if the proposal is appropriate based on the IBC and ADA requirements.

8. There is a small park/plaza located at the northwest corner of 10th Avenue NE and NE Franklin Drive, across the street from the covered structure. The proximity of the covered structure to the park/plaza would allow the park/plaza users to take advantage of the amenities offered at the covered structure, which would include a dog station, seating, bicycle parking, and a bicycle repair station. An easement would need to be granted to the City to ensure public access to the covered structure and its amenities.
9. A generator with horizontal wood slat enclosure on a concrete base and what appears to be chain link gates is proposed at the northwest corner of the site. As with all mechanical equipment, the enclosure will need to be fully screened from view, including sufficient height to screen from views from above. The proposed generator must use non-diesel fuel, and noise attenuation may be required to comply with the City's noise standards. As the proposed wood slat enclosure may not provide sufficient noise attenuation and the chain link gates would not fully screen the generator unit, these materials would not be appropriate for the enclosure.
10. Parking lot standards require drive aisles of a certain width to ensure cars have sufficient distance to back out of parking stalls. Where cars are not backing out, but using the drive aisle solely as a travel lane, the paved width shall meet minimum travel lane dimensions. The minimum travel lane width, for two-way traffic, is 20 feet if there are no vehicles backing up. Drive aisle widths for standard and compact stalls vary: 24 feet and 22 feet respectively; however, these are often mixed together. An average of 24 feet allows for consistency and

predictability while also meeting Appendix O's (Parking) purpose of economy of space. Appendix O also specifies parking stall dimensions.

11. All internal pedestrian walkways are required to maintain a minimum clear area at least 5 feet in width, except walkways to individual front doors or individual patios serving one residence may be 3 feet wide. Should up to two feet of the walkway in front of the two parking stalls in the entry courtyard be used for vehicular parking overhang, the walkway would need to be widened accordingly to maintain a minimum clear area of at least 5 feet in width excluding the vehicular parking overhang.
12. Issaquah Highlands is a pedestrian friendly and pedestrian oriented community where pedestrians receive higher priority than vehicles. Where cars and pedestrians cross paths, such as at vehicular drives, the design of these junctures must convey pedestrians' priority and minimize the impacts to pedestrians. Finally curb ramps should be in line with the pedestrian route where possible.
13. Another element in a pedestrian-friendly or -oriented community is visual interest. Blank walls create a dull pedestrian environment, discouraging pedestrian activity. Thus, avoiding unrelieved expanses of wall is necessary. Also, Appendix S (Urban Design Guidelines) states: "Blank walls are to be avoided; ...if necessary, articulation or other features should be provided." The proposed plans show blank walls on the north and south elevations of the parking garage, on the south elevation at the trash room, on the east elevation north of the pool, and the north-facing wall in the entry courtyard. Additionally, large prominent doors proposed for the trash room and both garage entrances along NE Falls Drive need to be designed to be visually interesting to improve the pedestrian experience. These and any other blank walls will need to be treated with visual relief and reviewed for compliance with Appendix S as part of Building Permit review. Vertical art elements are proposed at both ends of all three driveways, which would add visual interest to the streetscape, and their locations and design will be reviewed as part of Site Work or Landscape Permit review.
14. The proposal is largely consistent with many of the relevant Urban Design Guidelines (UDG) in Appendix S for the Traditional Townscape Neighborhood with the Multi-Family Overlay District. Key areas where the proposed plan is in full conformance with the guidelines include the following:
 - the Traditional Townscape guidelines encourage "buildings to be set immediately adjacent or in close proximity to public space" and for a "substantial portion of the building width [to be] at the "set-to" line." The main building forms a continuous streetwall along NE Falls Drive. Along 10th Avenue NE, the corner of the building is set close to the street, with individual unit entries opening onto the street. The northern wing of the building features the main building's entry and individual unit entries fronting on the street; however, this portion of the building is set back between 49 to 86 feet from the sidewalk. A detached covered structure at the northwest corner is set close to the 10th Avenue NE frontage, and helps to establish and continue the streetwall where the main building steps back;
 - the Traditional Townscape guidelines encourage minimizing driveways in order to minimize interruptions to the street-level frontage, and for street-facing garages to be set back from the building "set-to" line. Driveway cuts are kept to a minimum, with three driveways serving the site. Two garage entrances along Falls Drive NE provide access to the parking garage tucked under the building. As shown on the plans, these garage entrances would be set back approximately 24 feet from the sidewalk to minimize their presence on the street. This setback would also allow one vehicle to wait in front of

- the garage door while it opens, without blocking the sidewalk and street. No garage ramp slope information has been provided. Garage ramps near the driveway would need to be designed with a slope that would provide the driver adequate sight lines of oncoming pedestrians and vehicular traffic to ensure safety; and,
- in the Multi-Family Overlay guidelines, private park space should “accommodate a variety of activities for differing age groups.” Currently the design of the common areas suggests amenities that would accommodate both adults and children, including a pool, grill station, seating/dining areas, fire pit, play mounds, and lighted seating cubes.

There are a few aspects of the proposal that are not entirely consistent with the UDG:

- the Multi-Family Overlay guidelines state: “The architecture should be street friendly such as using townhouse style unit entries on the perimeter, and landscaping or design features to mask the parking portions of the building.” While the proposed plans show individual townhouse style residential units with entries facing NE 10th Avenue, utility rooms and garages are shown along NE Falls Drive with little residential presence on this frontage. In order to achieve a more pedestrian friendly frontage, at least a residential entry that establishes residential presence needs to be provided along NE Falls Drive, so that this frontage is not just driven by service/utility uses; and,
 - in the Multi-Family Overlay, the guidelines state: “residents should have both private and public outdoor space.” This means the applicant should provide balconies or private outdoor space for most units. While some balconies and decks are shown on the elevations and landscape plans show a shared roof deck, detailed plans are not provided regarding decks/balconies.
15. The landscape is regulated by Appendices P (Landscaping) and S (Urban Design Guidelines). Parking lot landscape requirements for treatment of the edges of parking areas would apply to the surface parking area in the entry courtyard. A minimum landscape buffer of 3 feet in height and width planted with hedges is required to be planted to provide a more pleasant and defined edge for pedestrians adjacent to parking areas. The edge landscaping for the parking spaces in the entry courtyard will be verified with Landscape Permit review.
16. The Traditional Townscape landscape is generally formal, symmetrical, and evergreen. Landscape screening is only expected between incompatible uses and no buffers are necessary between any uses in Development Area 4; therefore, the landscape between properties should provide a transition between them and tie them together as the preliminary landscape plans appear to do. Appendix A (Goals and Commitments) talks about “...accommodat[ing] individual expression within an overall continuity and a sense of shared responsibility toward the street, neighborhood, community and environment.” The landscape transition reflects this shared responsibility.

Elements of the proposed landscape plan that are not in consistent with Traditional Townscape include the following:

- the proposed plantings along the western and southern portion of the site, which require more formal plantings that are symmetrical and help define the edges; and,
- the proposed evergreen trees along the wetlands which are appropriate adjacent to wetlands and ponds, but which will not be consistent with the Traditional Townscape character on the interior of the site.

During Landscape Permit review, DSD will review landscape materials for consistency with Appendices P and S.

17. There are existing street trees along NE Falls Drive and 10th Avenue NE. These trees are within the right-of-way. The City has an interest in not only protecting its property but maintaining the value inherent in these trees due to their size and maturity. Damage to the street trees might necessitate their replacement. With the exception of two trees along NE Falls Drive and one tree along 10th Avenue NE that will be removed in order to accommodate proposed driveways, all existing street trees are proposed to be retained. The applicant shall plant new street trees near the driveways along NE Falls Drive, such as through landscape bulb-outs, to maintain the existing street tree spacing to the extent possible.
18. Appendix E (Critical Areas) has many specific requirements which are noted in the conditions, e.g. permanent survey markers, signs along buffers, limitations on chemicals. Certification of boundaries following construction ensures that the critical area boundaries are respected during construction, while fences protect the critical area from both people and pets after occupancy.
19. While the City allows mailboxes within buildings, the United States Postal Service (USPS) has not allowed mailboxes within buildings except for those necessary to meet ADA requirements. An external kiosk will likely be required. Mailboxes are currently shown in the entry courtyard on the exterior of the building. The mailboxes and any associated external kiosk would need to be integrated and compatible with the design of the building.
20. Driveway widths are one factor in vehicular driving speeds. The driveways should be as narrow as possible to function, while promoting low travel speeds as encouraged by Appendix A. A narrow driveway (i.e., 20 feet), will create a safer environment for pedestrians negotiating the site.
21. The Development Agreement does not provide different standards for private and public facilities; therefore, any elements near right-of-way must meet certain minimum design requirements, to ensure that plant material doesn't impact public facilities. Thus, root barriers must be installed adjacent to public facilities.
22. Identifying wet and dry utility vaults, meters, equipment, and appurtenances now is just as critical as identifying buildings, landscape, pedestrian facilities, roads, and other elements as last minute additions or modifications can have significant site-wide implications. Changes to buildings, landscape, pedestrian facilities, and roads likewise will require permit modification.
23. Access and equipment associated with wet and dry utilities are a necessary part of a functioning community. However, their presence does not enhance the overall project character and objectives. Screening and location can reduce the impact of these necessary elements.
24. At Issaquah Highlands, sidewalks are 5 feet wide. Generally, walkways throughout the development must meet this minimum width. While the proposed sidewalks appear to be wide enough, car overhangs, handrails, landscape can reduce the available width if not properly designed. At Site Work and/or Landscape Permit review, staff will confirm that a minimum of 5 feet of walkway width will be provided. Walkways narrower than 5 feet do not accommodate two people walking side by side, therefore the use of narrow walkways must be carefully selected based on the walkways likely use. The use of two steps together improves people's ability to notice the steps and thus prevent tripping. Consistent with sidewalks, the walkways must be constructed in concrete.

25. An on-site trail along the wetland is proposed which would provide a pedestrian connection between 10th Avenue NE and the WSDOT pond trail, and an easement would need to be granted to the City to ensure public access of this trail. The trail will be a minimum of 5 feet in width, and may be widened to 12 feet to accommodate bicycles. Gravel is a material usually used in critical areas or less formal projects, such as adjacent to the wetland and pond. Gravel or crushed rock surfacing will need to be located a minimum of 20 feet away from any right-of-way and the WSDOT pond trail in order to minimize the migration of loose material on paved public paths. While there are some locations where gravel paths are appropriate, a dense urban environment, such as the interior of the site, is not one of them.
26. Little lighting information was provided. Lighting review will occur with construction permit review. Appendix A's goals envision that this will be a sustainable community both in design and construction. A key aspect of achieving sustainability is lighting efficiency through the placement, design, and amount of light for the development. Finally, as a garage may have lights on 24 hours a day, it is necessary to design the light to minimize impacts on adjacent areas, such as the wetland.
27. Based on the number of floors in the building, its height is less than the 85 feet allowed. This will be confirmed with Building Permit review.
28. Little information on signage was provided and a separate Sign Permit is always required.
29. The proposal is within the Project Envelope for Issaquah Highlands; therefore, no further SEPA review is required per Appendix I (SEPA).
30. Copies of the submittal were distributed to City departments and Eastside Fire and Rescue for review. Comments received have been incorporated into the approval and construction conditions, as appropriate.
31. Public notice was distributed to properties within 300 feet of this application. No comments were received.
32. The proposal was reviewed and approved by the IHCA ARC. The IHCA ARC's approval letter dated April 14, 2017 is included in Attachment 2.


Jean Lin, DSD Senior Planner

October 13, 2017
Date

Attachment List:

1. Revised site plan received via email on September 6, 2017, elevations dated July 11, 2017, and preliminary landscape plans dated August 1, 2017
2. Architectural Review Committee approval letter, dated April 14, 2017
3. Construction Conditions

cc: Daisuke Zaoya, Sumitomo Forestry America
Darcy Perrault, Clark Design Group
Lindsay Fallert, Clark Design Group
Jason Book-Symons, Trammell Crow Residential
Lucy Sloman, DSD Land Development Manager
Doug Schlepp, DSD Engineering Consultant
Sarah Hoey, Issaquah Highlands Community Association



10TH AVE NE & NE FALLS DRIVE
ISSAQUAH WA 98029

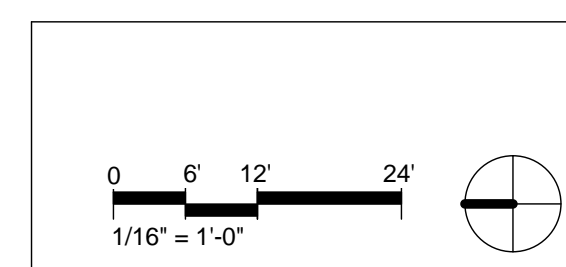
REVISIONS:

NO.	DATE	DESCRIPTION
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SITE PLAN

SHEET NO: _____

A0.01


$$1/16'' = 1'-0''$$

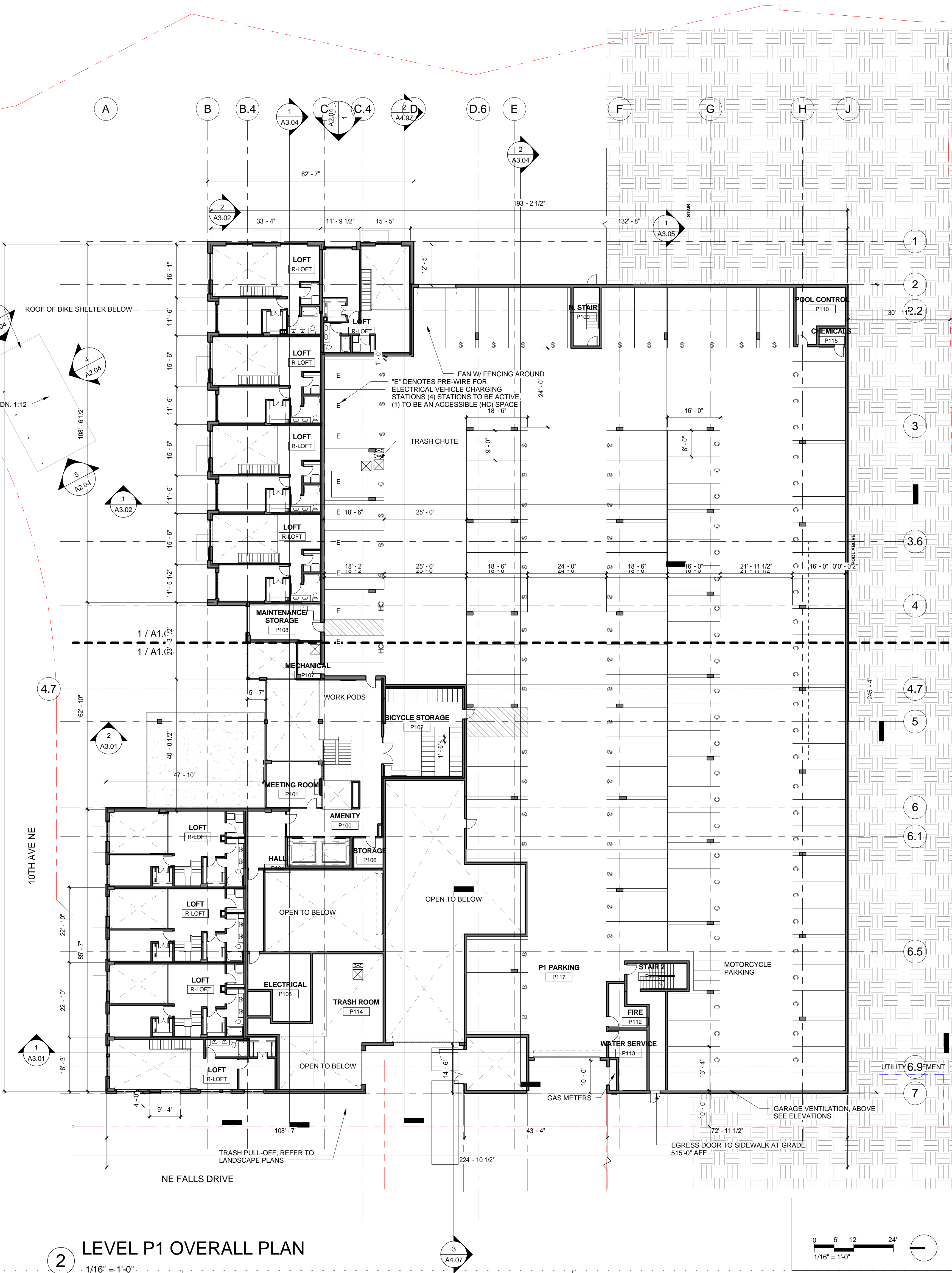
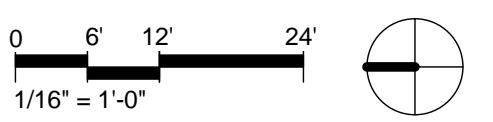
BLOCK 19
10TH AVE NE & NE FALLS DRIVE

REVISIONS:

LEVEL P1 & P2 OVERALL FLOOR PLAN

SHEET NO: _____

A1.01



EXTERIOR MATERIAL NOTES	EXTERIOR MATERIAL ALTERNATES	
1. "B" AT VINYL WINDOWS NOTE BLACK EXTERIOR FRAMES. ALL OTHER VINYL WINDOWS TO BE WHITE. 2. METAL COPING, EXTERIOR TRIM, AND VENT SHROUDS TO MATCH ADJACENT WALL COLOR, U.N.O. 3. METAL CANOPIES AND DECKS TO MATCH AEP SPAN COLOR, U.N.O. 4. METAL CANOPY AT BUILDING ENTRANCE TO HAVE WOOD OR WOOD LOOK SOFFIT. 5. OVERHEAD WEATHER PROTECTION INCLUDES THE PORTE COCHERE, 4' DEEP CANOPIES AT TOWNHOUSE ENTRANCES, AND 10' SETBACKS AT PARKING GARAGE ENTRANCES.	THE MATERIALS BELOW ARE TO BE PRICED AS ALTERNATES. CONTRACTOR TO VERIFY FINAL SELECTION PRIOR TO PURCHASE OF EXTERIOR MATERIALS. FC-1 CERACLAD FIOTTO PEARL FC-2 NICHHA ARCHITECTURAL BLOCK GRAY M-1 OPTION 1: AEP SPAN COOL BLACK RAWHIDE 12" WITH RIB M-1 OPTION 2: CERACLAD CAST STRIPE CHARCOAL W-1 CERACLAD RUSTIC WOOD CARAMEL W-2 NICHHA ROUGHSAWN SMOKE	
		- KEYNOTE LEGEND - B-1 BRICK: MUTUAL MATERIALS, COLOR: COAL CREEK, SIZE: ECON, WITH LATICRETE GROUT, COLOR: 78 STERLING SILVER C-1 CONCRETE: BOARD FORMED CONCRETE C-2 CONCRETE: SACK AND PATCH CONCRETE FC-1 FIBER CEMENT: WOODTONE WHITE GRANITE FC-2 FIBER CEMENT: HARDIE REVEAL, PAINTED COLOR TO MATCH SHERWIN WILLIAMS SW 9171 FELTED WOOL M-1 METAL PANEL: OPTION 1: AEP SPAN, PRESTIGE SERIES 1 RIB, COLOR: COOL MATTE BLACK W-1 WOOD LOOK: WOODTONE SMOKEY ALDER W-2 WOOD LOOK: WOODTONE COASTAL GRAY



1 WEST ELEVATION



16 SOUTH ELEVATION

ALEXAN HIGHLANDS
BLOCK 19
10TH AVE NE & NE FALLS DRIVE
ISSAQUAH, WA 98029

NOT FOR CONSTRUCTION

DATE: 07/11/17
DWN BY: LF, DP
JOB NO. 16-27

REVISIONS:
NO. DATE DESCRIPTION

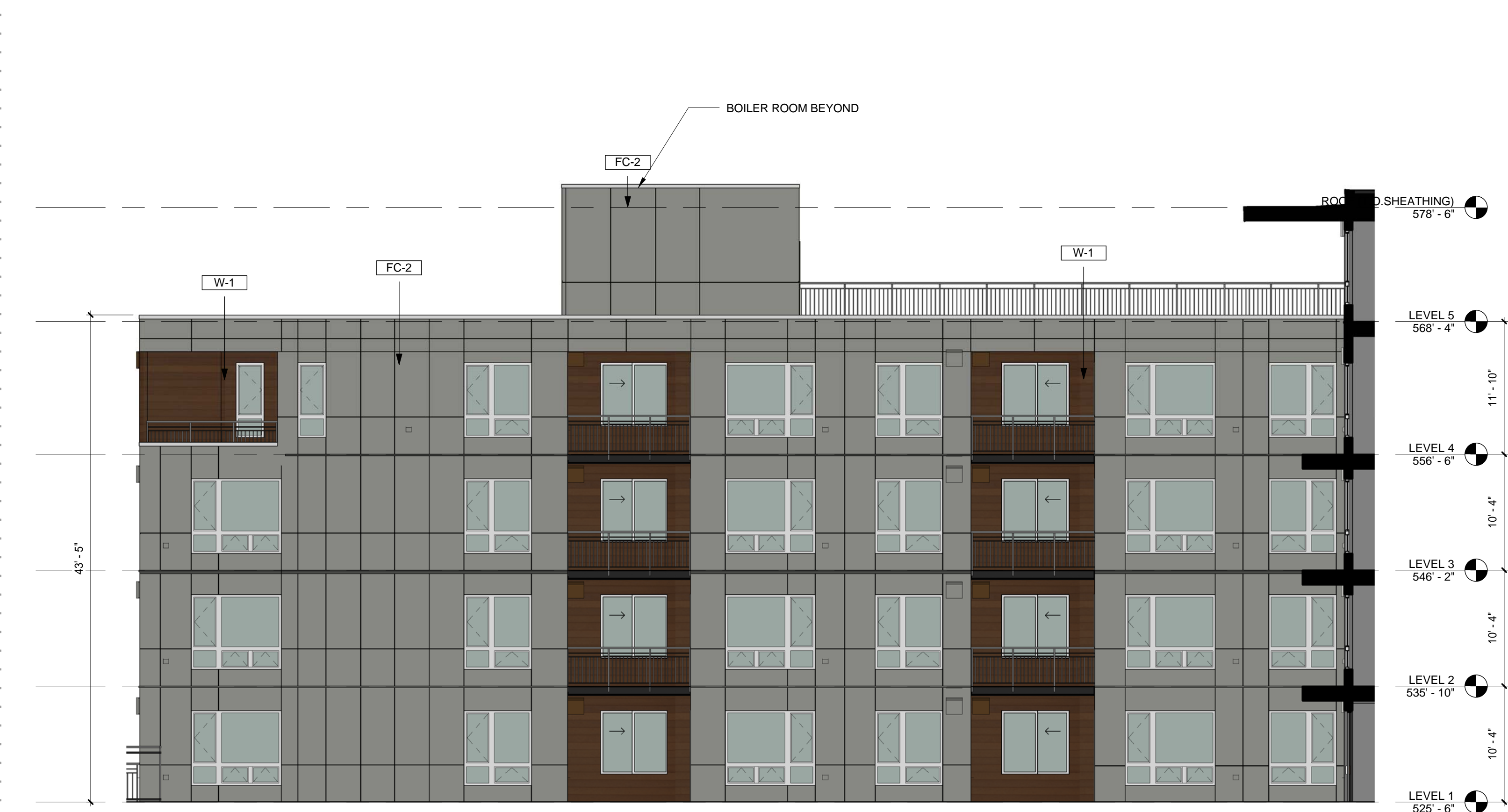
ISSUE:
ARC REVIEW

ELEVATIONS

SHEET NO.

A2.01

EXTERIOR MATERIAL NOTES	EXTERIOR MATERIAL ALTERNATES	
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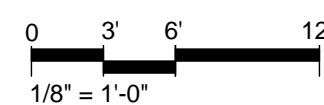
1 COURTYARD NORTH ELEVATION
1/8" = 1'-0"



2 COURTYARD SOUTH ELEVATION
1/8" = 1'-0"



16 EAST ELEVATION
1/8" = 1'-0"



NOT FOR CONSTRUCTION

DATE: 07/11/17
DWN BY: LF, DP
JOB NO. 16-27

REVISIONS:
NO. DATE DESCRIPTION

ISSUE:
ARC REVIEW

ELEVATIONS

SHEET NO.

A2.03

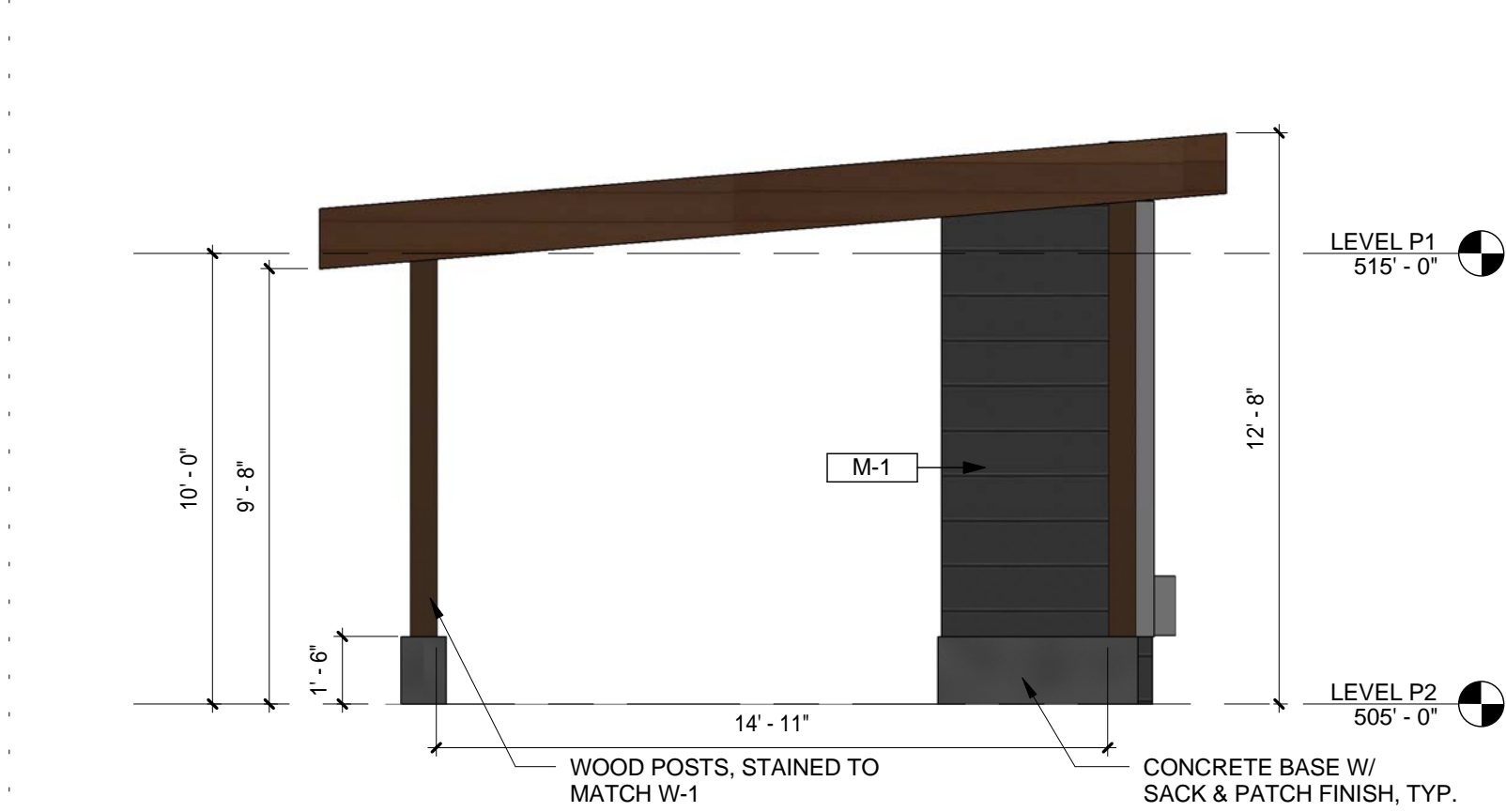
ALEXAN HIGHLANDS
BLOCK 19
10TH AVE NE & NE FALLS DRIVE
ISSAQUAH, WA 98029

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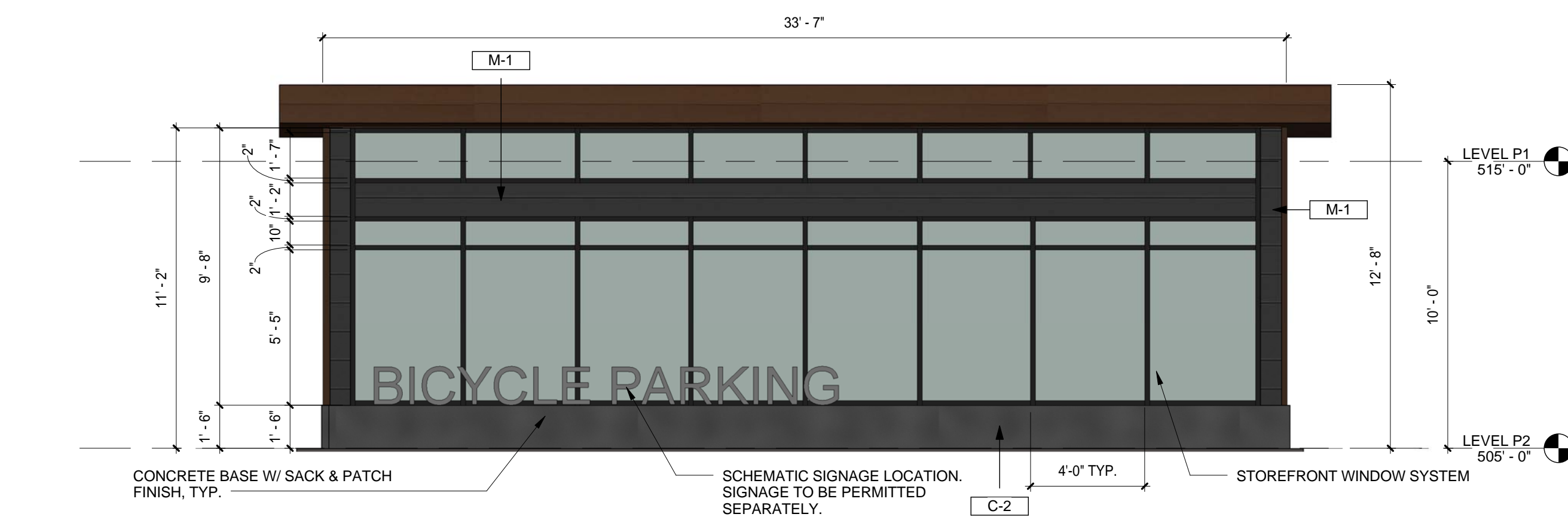
CLARK
Design Group PLLC
1401 West Garfield St.
Issaquah, WA 98029
206.782.8208 F.206.782.7818



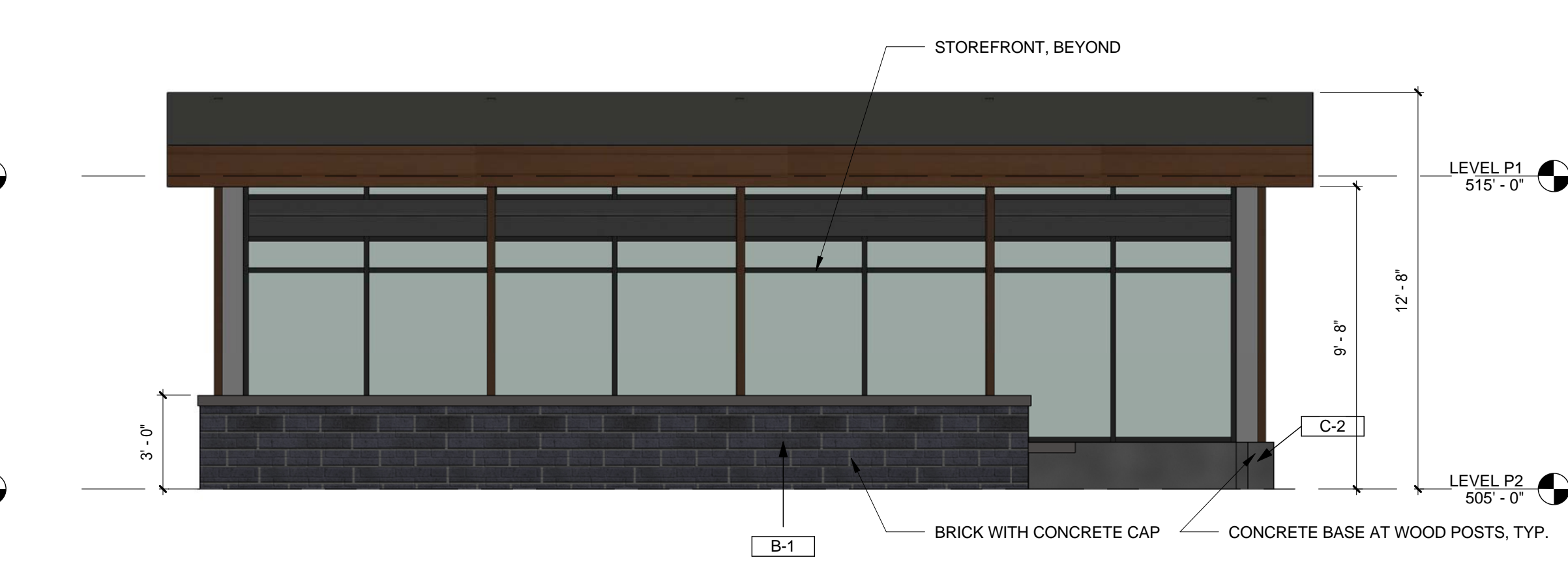
1 NORTH ELEVATION
1/8" = 1'-0"



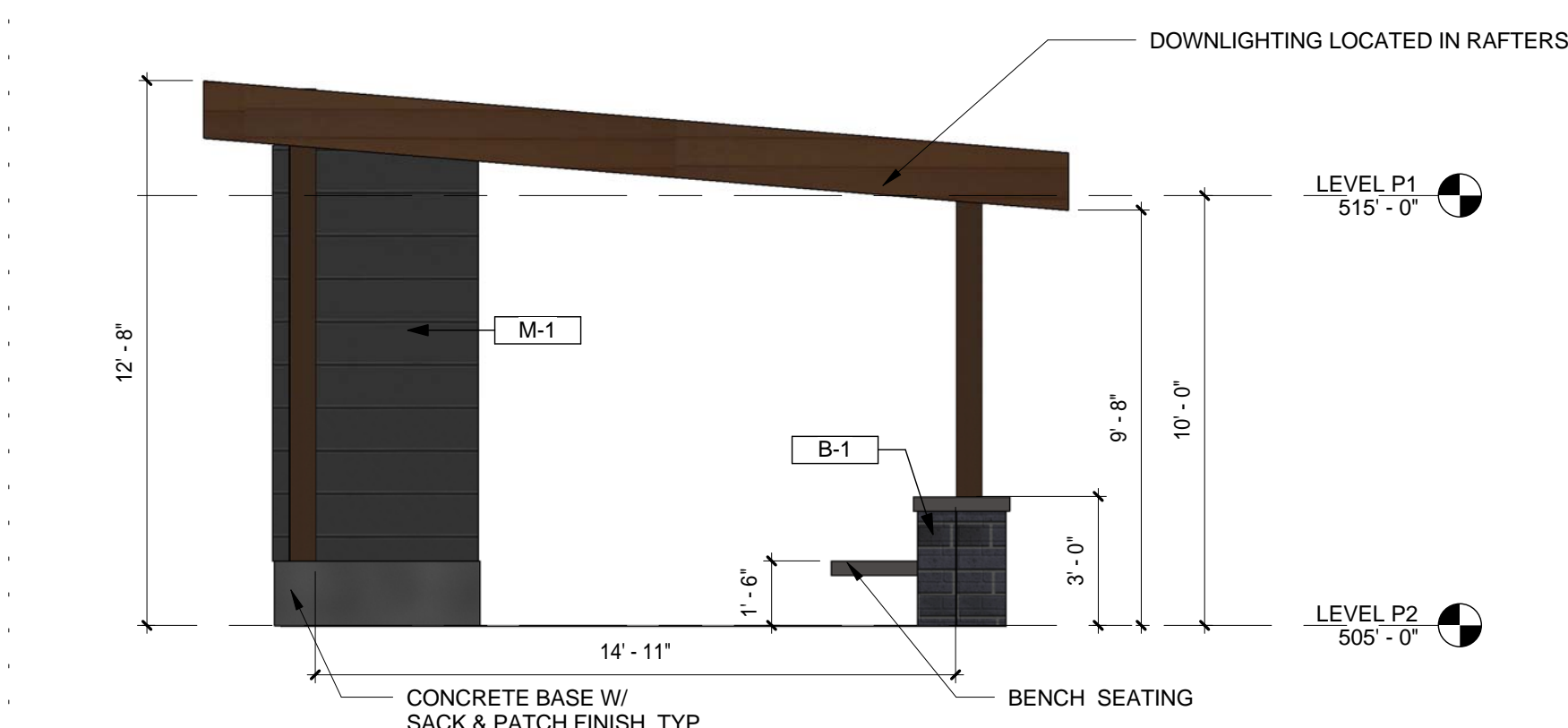
2 NORTH BICYCLE PARKING
1/4" = 1'-0"



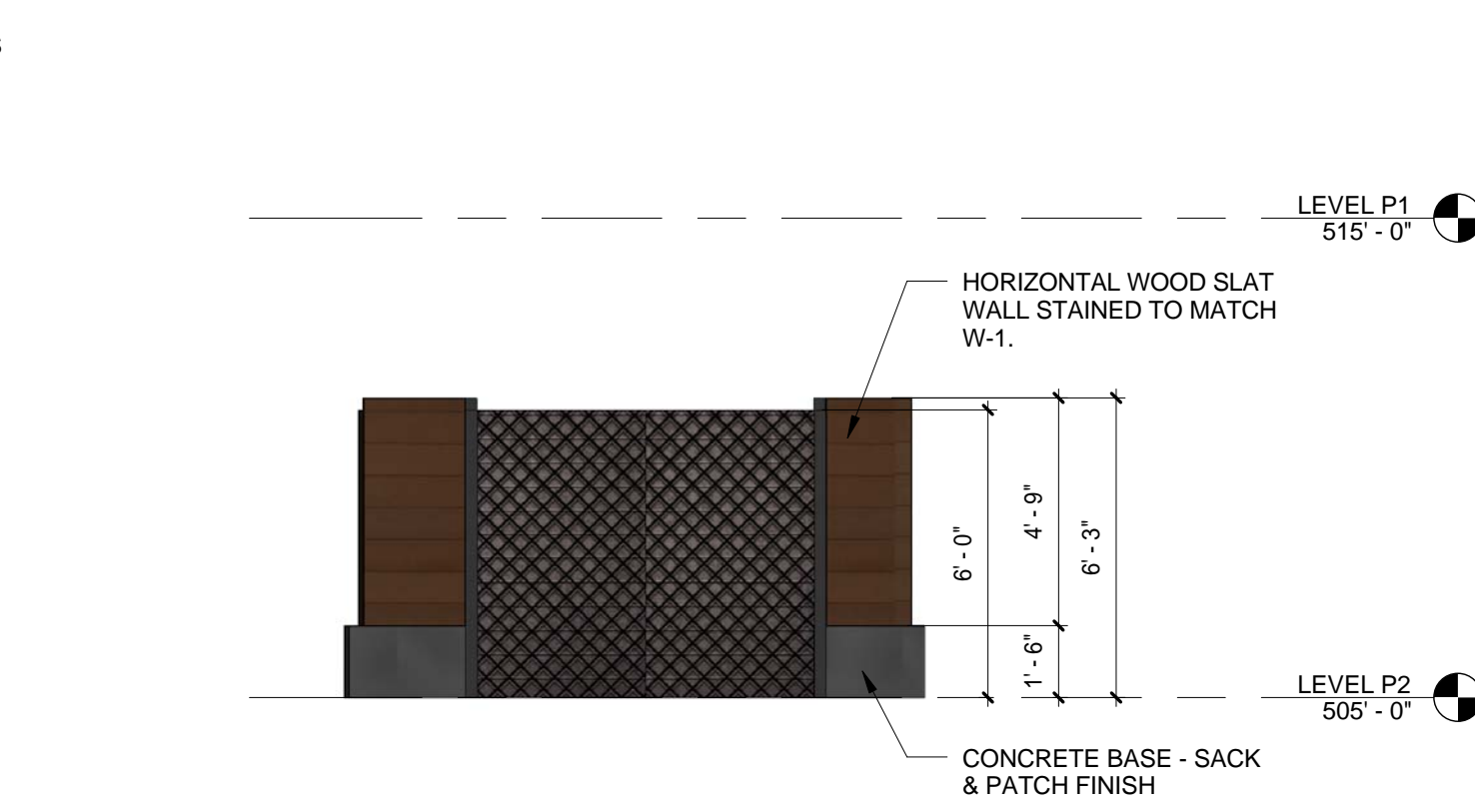
3 WEST BICYCLE PARKING
1/4" = 1'-0"



4 EAST BICYCLE PARKING
1/4" = 1'-0"



5 SOUTH BICYCLE PARKING
1/4" = 1'-0"



6 SOUTH GENERATOR SCREENING
1/4" = 1'-0"



- NUMBERED NOTES (LEVEL P2)
- 1 CIP CONCRETE SITE WALL
 - 2 CIP CONCRETE STAIRS
 - 3 WOOD RAIL FENCE - SEE 4/L5.01
 - 4 BIKE RACK - SEE 5/L5.01
 - 5 EXISTING SIDEWALK TO REMAIN
 - 6 INFILL SIDEWALK PAVING
 - 7 STRUCTURE - SEE ARCHITECTURAL
 - 8 UNIT PATIO
 - 9 MONUMENT SIGN
 - 10 CIP SEATWALL, TYP
 - 11 VERTICAL ART ELEMENT
 - 12 CONCRETE DRIVEWAY - SEE CIVIL
 - 13 TRASH HOLDING AREA - CONCRETE PAVEMENT
 - 14 CONCRETE WALK TO TRANSFORMER ROOM
 - 15 3' WIDE CONCRETE WALK TO UNIT PATIO
 - 16 5' WIDE CONCRETE WALK
 - 17 5' WIDE CRUSHED ROCK TRAIL WITH STEEL EDGING
 - 18 5' WIDE ASPHALT TRAIL
 - 19 3' WIDE CRUSHED ROCK TRAIL TO UNIT PATIO
 - 20 GENERATOR PAD AND SCREEN FENCE - SEE ARCHITECTURAL
 - 21 PLAY MOUNDS - SEE 9/L5.01
 - 22 PLANTED JOINT
 - 23 BOCCIE BALL LAWN
 - 24 BENCH
 - 25 MAILBOXES
 - 26 ACCESSIBLE RAMP @ 8% SLOPE
 - 27 WALK @ 5% SLOPE
 - 28 STAIR DOWN FROM LEVEL 1

NOT FOR CONSTRUCTION

communita
atelier

1402 3rd Ave Suite 1000
Seattle WA 98101

DATE: 08/01/17
DWN BY: AS LN
CHK BY: AS
JOB NO. 16-27
DPD NO.

REVISIONS:

ISSUE: 50% DD SET

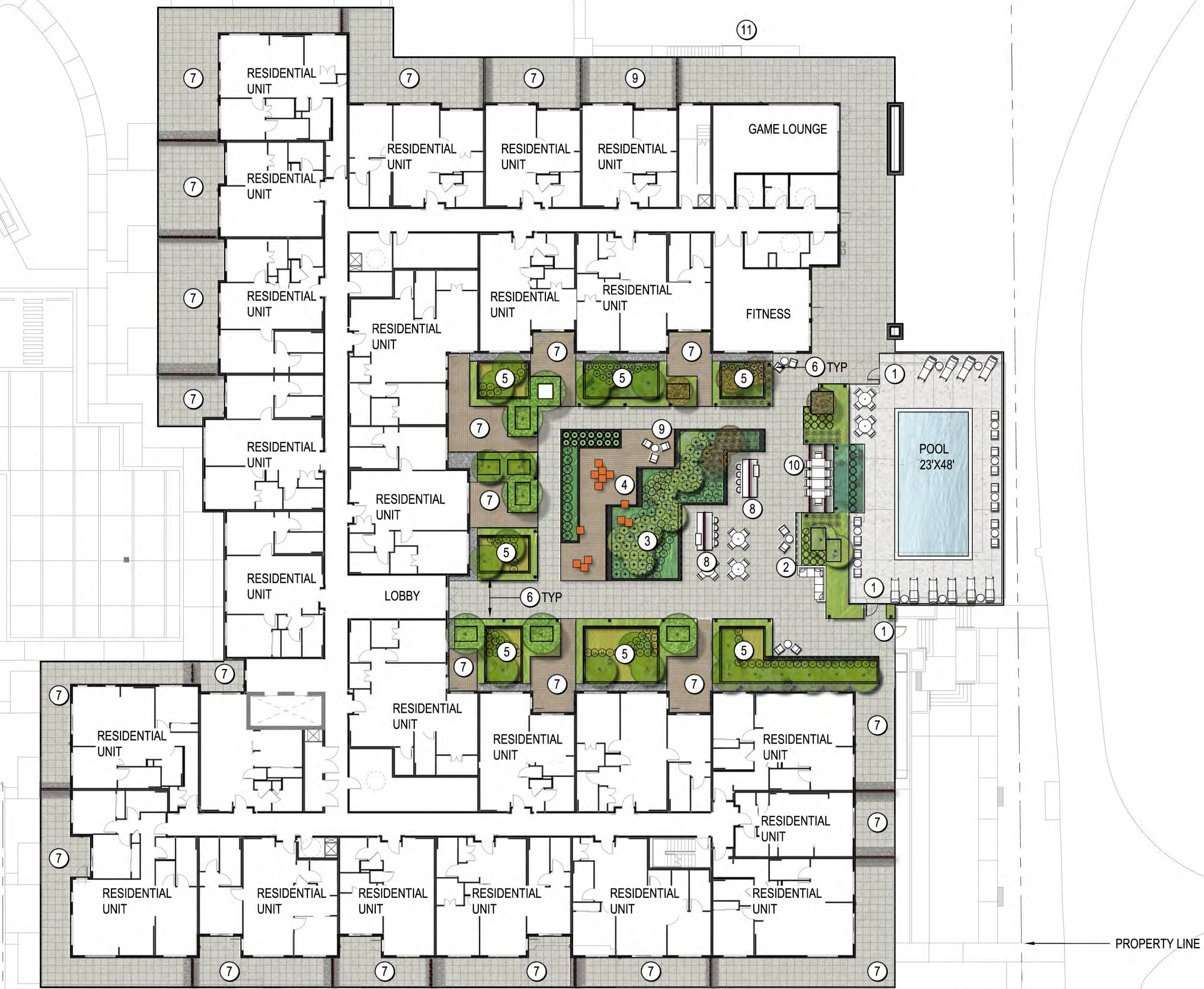
PLANTING PLAN
LEVEL P2

SHEET NO:
L1.51

ISSAQUAH HIGHLANDS
BLOCK 19
10TH AVE NE & NE FALLS DRIVE
SEATTLE, WA 98029

CLARK
1401 West Corfield St
Seattle, Washington 98119
T: 206.762.2208 F: 206.762.2116

1 PLANTING PLAN - LEVEL 1
SCALE: 1"=20'-0"



- NUMBERED NOTES (LEVEL 1)
- 1 LOCKING GATE - SEE 2/L5.01
 - 2 CIP CONCRETE SEAT WALL - 18" HEIGHT - SEE 8/L5.01
 - 3 BERMED PLANTER
 - 4 LIGHTED SEATING CUBES - SEE 20/L5.01
 - 5 CIP CONCRETE PLANTER - 24' HEIGHT
 - 6 LOW STEEL/ALUMINUM PLANTER EDGE - 12" HEIGHT
 - 7 UNIT PATIO
 - 8 GRILL STATION WITH BAR SEATING - SEE 18/L5.01
 - 9 PREMANUFACTURED GAS FIRE PIT
 - 10 DINING AREA TRELLIS - SEE 16/L5.01
 - 11 STAIR DOWN

NOT FOR CONSTRUCTION

communita
atelier
1402 3rd Ave Suite 1000
Seattle WA 98101

DATE: 08/01/17
DWG BY: AS LN
CHK BY: AS
JOB NO: 16-27
DPD NO:

REVISIONS:

ISSUE: 50% DD SET

PLANTING PLAN
LEVEL 1

SHEET NO:
L1.52

ISSAQUAH HIGHLANDS
BLOCK 19
10TH AVE NE & NE FALLS DRIVE
SEATTLE, WA 98029

CLARK
1401 West Garfield St
Seattle, Washington 98119
P: 206.922.2267 F: 206.922.2116

April 14, 2017

Clark Design Group PLLC
Lindsay Fallert
1401 West Garfield Street
Seattle, WA 98119

RE: IHCA Architecture Review Committee (ARC) review of Block 19 Pre- ASDP application

Dear Lindsay,

Thank you for your architectural presentation for the Block 19 multifamily project. The ARC Committee has reviewed your submittal which included site planning, height and massing, architectural expression, shown use of pedestrian, environmental and vehicular uses, conceptual building materials and colors and conceptual landscaping. The ARC committee approves your plans as provided and you have our approved to move forward with the ASDP application process with the City of Issaquah. Summitomo will be required to follow the multi-family Residential Architectural Standards for the Issaquah Highlands which includes submittal of as-built plans, a project board of exterior colors and materials, as well as landscaping plans for the multifamily common areas. Pre-construction meetings will be required as well as post framing, siding and final ARC inspections which are part of the required process.

Please note this approval is for ARC purposes only and additional permitting and approvals will be necessary from the City of Issaquah. This approval does not replace Federal, State or City regulations; or City of Issaquah building code; or other applicable regulations used by the City of Issaquah in its permit review of projects within Issaquah Highlands.

We look forward to working with you on this project. Please let us know if you have any questions.

Sincerely,

Sarah Hoey

Sarah Hoey
Executive Director
Issaquah Highlands Community Association
Sarah.h@ihcommunity.org
425-507-1120

cc:

David S. Meade, AIA, Issaquah Highlands ARC (via email)
Dennis Riebe, Issaquah Highlands ARC (via email)
Lucy Sloman, AICP Land development manager City of Issaquah (via email)

Attachment 3 – Construction Conditions

C1. The permit plans must justify sizing of the building based on current building code requirements. The storm pond property to the east is not permitted to be used in determining the allowable area increase for this project. It is not to be considered as open space or a public way because it is not dedicated for public use.

C2. A determination for use of the wetland was not made at this time. If there is a potential to consider the wetland as an open space, the Building Official will need to be consulted for a thorough evaluation with justification from the design team.

C3. Water Mains & System:

- a. All water main flow interruptions shall be scheduled with City PWO at least 14 calendar days ahead. All flow interruptions, individually, shall be no longer than 8-consecutive hours leaving 16-consecutive hours for storage recovery. These instructions are in addition to, or modification to, existing City Standard Operating Procedures for scheduled water main shut-downs.
- b. All water main taps on City-owned water mains greater than 2" shall be made by licensed, "wet-tap," third part vendor. The third-party vendor shall be City-approved and their primary business is tapping live, in-service public water mains.
- c. Provide deeded easement(s) for all water mains and appurtenances.
- d. Abandon all unused water main stubs on or facing parcel. Abandon stubs at existing water main, removing the stubbed valve, pipe and valve box.
- e. Do not plan trees within 10' of any water main, water service line or water meter.
- f. Provide detailed water and sewer utility drawings.

C4. Standard Details:

- a. Show all applicable Water Standard Details on design drawings.
- b. Use this product and application method for fire hydrant Standard Detail W-02, Blow Off Assembly Standard Detail W-04 and Air Vacuum Release Valve Assembly Standard Detail W-07: "Clean, scuff-sand and prepare per paint manufacture's specification. Prime with 1 coat (6 Mills) of Sherwin Williams "Pro-Cryl" Acrylic Primer (B66 W 310 643-22681) and then top-coat with 1 coat (6 Mills) of Sherwin Williams "Cryl-HPA" High Performance Acrylic Gloss, High Gloss Bright White (B66 W 377 6405-18908)."
- c. Add this comment to Water Standard Detail W-04, Item #E, Blow Off Assembly: "Install brass, 4" female Seattle Standard Thread x 2 1/2" male National Standard Thread fitting. Install 2 1/2" cap NST."
- d. Do not install components #M - #Q, inclusive, for Water Standard Detail W-11, 3" & Larger Meter Installation.

C5. Water Meters:

- a. Double meters (or more) are preferred for domestic water. No more than 10% of domestic water meters shall be single meters. This applies to 3/4" – 1" domestic meters.
- b. Install brass address tag attached to each setter, for all water meters, whether new or existing, within the physical boundaries of this project.
- c. Meters 2.5" and larger: Developer to plumb from main to meter, install vault and associated piping per Standard Detail. Developer to supply and install meter. DSD to

assure meter and meter bypass are off and not used until PWO inspects installation and Backflow Assembly installation. DSD to inform PWO when meter is installed. Developer to be charged for PWO inspection fee and Meter Transmitter Unit per IMC, in addition to all other applicable fees.

- d. Please plan accordingly for PWO installation of new water meters and services .75" through 2". Lead time is minimum of eight weeks. Final grade elevation at sidewalk or planter where meters are to be installed must be established before City provides installation. Work directly with PWO at (425) 837-3470.
- e. Show landscape irrigation water meter. Provide landscape plan showing irrigation connection to potable water, including meter and backflow device.
- f. Provide detail of domestic water meter(s).

C6. Fire Suppression Systems:

- a. Define if fire suppression system(s) will be installed. If so, define, detail and show supply, and how it shall be metered. Strict criteria exist for Flow-Through Fire Systems; if planned, please provide detailed plans for fire suppression system(s) to PWO and other required agencies for review, allowing sufficient time for agency review, comments and approval.
- b. The following elements define a flow-through fire system for (Drinking) Water Quality purposes; these elements must be included into the design without deviation:
 - i. No fire department pumper connection;
 - ii. Potable water piping and materials must be used;
 - iii. Piping end(s) terminate at a non-human-consumptive fixture using potable water (toilet, washing machine) as its sole water source;
 - iv. Piping arrangement cannot be looped;
 - v. Piping arrangement shall be one continuous pipe without branches or tees unless the branch or tee ends as described in item "iii."
 - vi. A single feed to an elevated floor is allowed.
- c. Do not install wallboard or other covering material on walls or ceilings where flow-through fire system piping is located until (Drinking) Water Quality inspection has been scheduled, conducted, and the piping arrangement approved.
- d. Do not deviate from flow-through fire system piping design, once approved by (Drinking) Water Quality and other agencies; install piping per approved design.

C7. Drinking Water Quality & Backflow Protection:

- a. Backflow devices may need to be installed (irrigation, fire, other). Backflow permits are separate permits, separate from all other permits. Multiple backflow devices can be on one permit.
- b. Water Quality inspections required at least at rough-in and final. Schedule inspections through MyPermit.Com. If installing systems connected to potable water such as radiant floor heating, flow-through fire suppression, water features, swimming pools, or installing non-potable water lines for irrigation or wastewater uses, these uses will require backflow protection individually and in addition to any site-required backflow protection. It is highly advisable to work directly, often and early during construction with City PWO (Drinking) Water Quality section. Not doing so may very well delay certificate of occupancy.

- c. This project will require a Reduced Pressure Backflow Assembly (RPBA) or a Reduced Pressure Detector Assembly (RPDA) installed immediately behind the/all water meter(s), whether domestic, irrigation or fire. Install RPBA (domestic & irrigation) and RPDA (fire) immediately behind all water meters for this property, regardless of water use or designation. Contact PWO (Drinking) Water Quality Section for questions and backflow device location questions. Detail assembly and show location on applicable Construction Drawings. Determination based on:
 - i. Building height in excess of 30'-0"
 - ii. Proposed swimming pool
 - iii. Proposed reclaimed water use on property or in property-bordering ROW (required, minimum backflow protection for all water sources from City water main is RP-style device.)
- d. All meters installed (dropped) for this project will be locked off immediately after installation and remain so until an inspection is conducted by PWO (Drinking) Water Quality personnel, and a record of successful testing of the backflow prevention device has been delivered. Meters will be unlocked by City after both of these conditions have been met.
- e. If off-site or on-site reclaimed water is collected, stored or used, a Reduced Pressure Backflow Assembly (RPBA) shall be installed above-ground and immediately behind all domestic and fire water meters associated with this project. It is highly recommended that these above-ground installations be protected from freeze and vandalism. Please work directly, often and early during construction with City PWO (Drinking) Water Quality section. Not doing so may very well delay certificate of occupancy.

C8. Include design details for all water-using appurtenances, appliances or amenities.